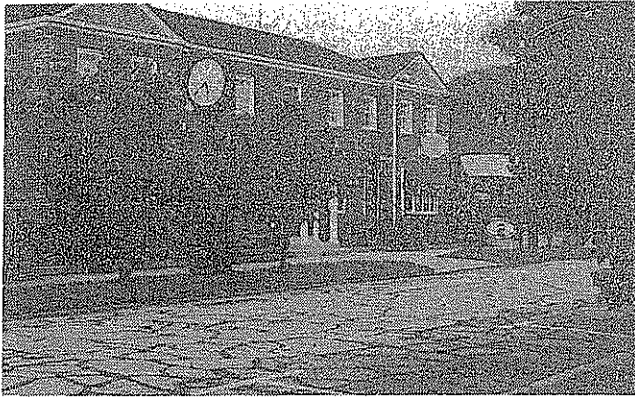


CHAPTER 2: GOALS FOR THE BILTMORE VILLAGE HISTORIC DISTRICT

ECONOMIC DEVELOPMENT GOALS	13
LAND USE AND PLANNING GOALS	15
HISTORIC PRESERVATION GOALS	16
SPECIFIC GOALS	19

ECONOMIC DEVELOPMENT GOALS:

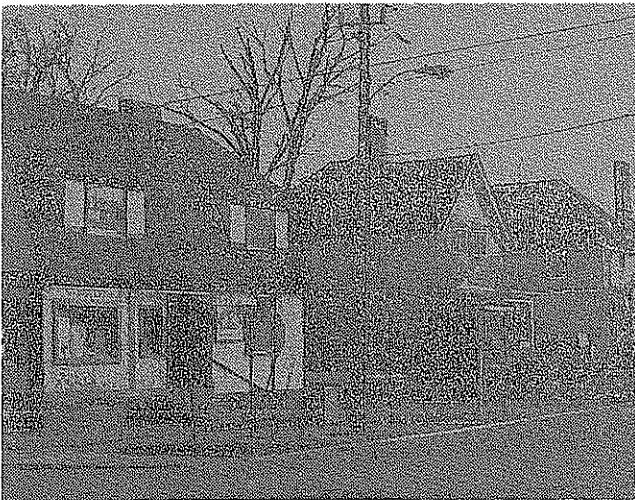
1. Provide a phased program for the immediate introduction of new public improvements in a manner that will complement and enhance the quality of the historic elements and the recent private improvements.



Re-pave streets



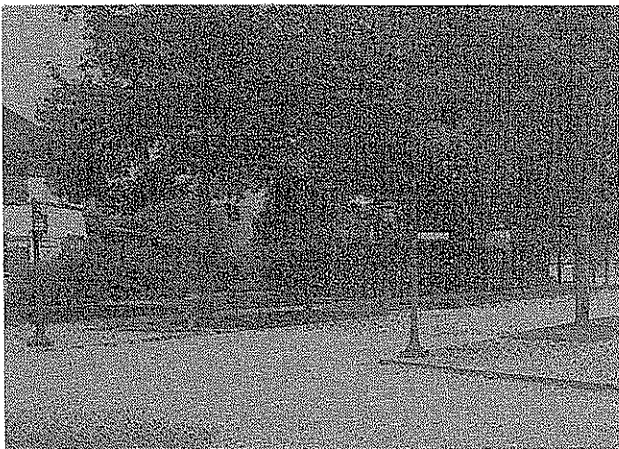
Provide sidewalks



Encourage underground wiring



Manage traffic



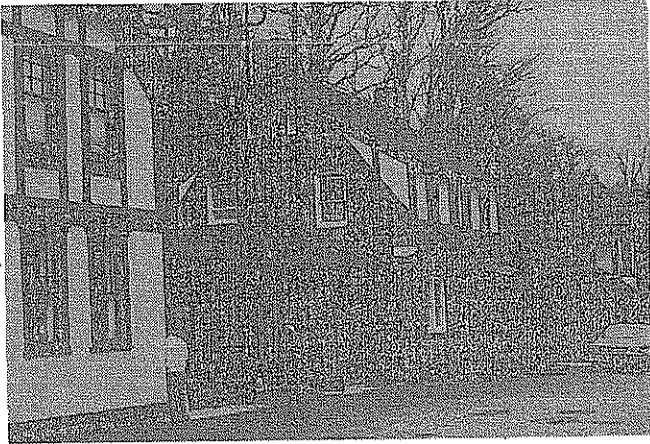
Add historic street lights



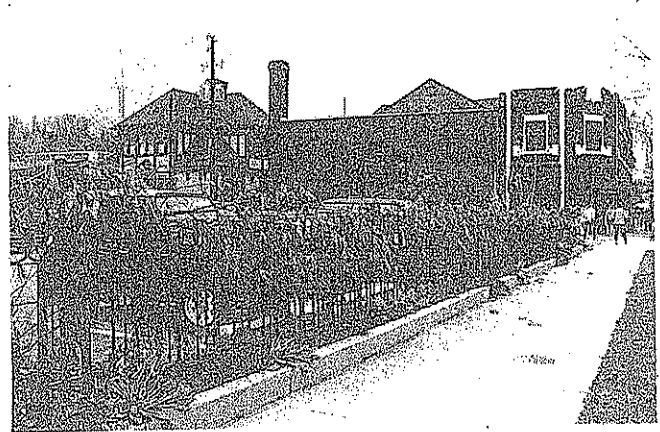
Encourage quality control

ECONOMIC DEVELOPMENT GOALS (CONT.)

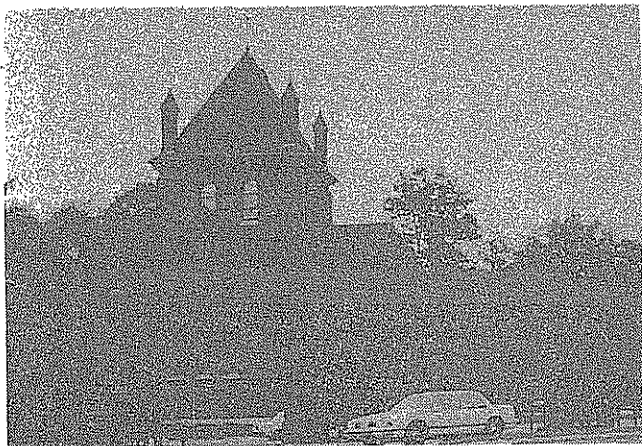
2. Understand, accept, and encourage the inevitable development in and around Biltmore Village. Develop a strong program to steer development in a direction that will enhance the pedestrian experience, build on the historic plan and maintain a high standard of quality.



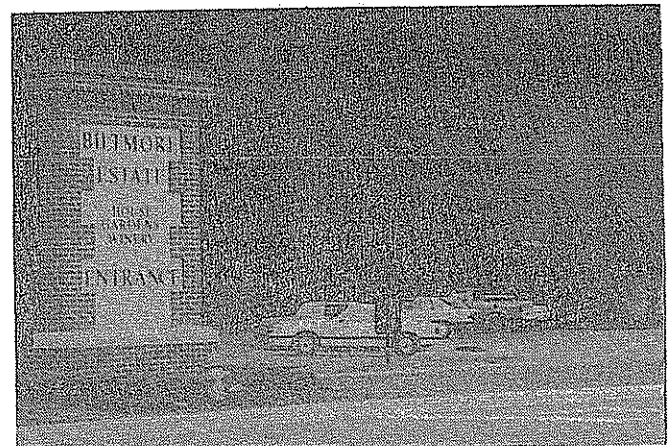
**Encourage commercial development
in a historic context**



Buffer parking



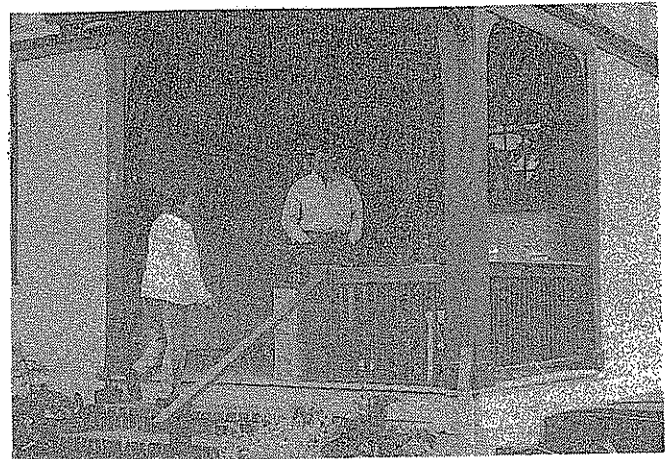
Support historic design guidelines



Encourage architectural signage

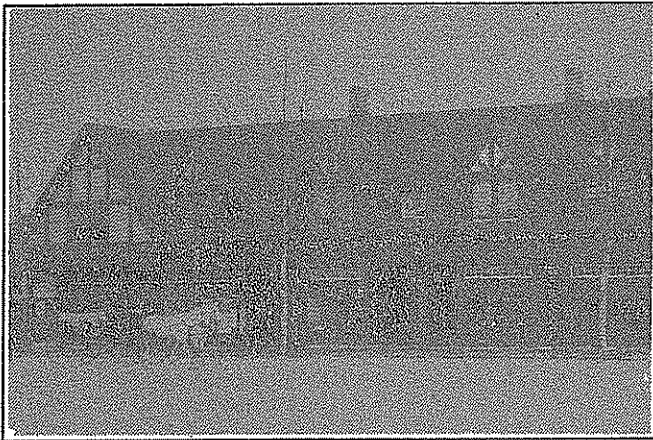


Orient businesses to the pedestrian



Re-use historic elements

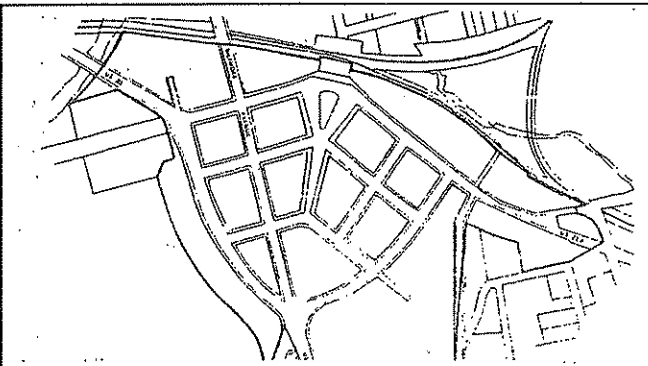
LAND USE AND PLANNING GOALS:



Original commercial building



Pre-1930 historic building



Biltmore Village Plan

1. Re-establish and strengthen the image of the original and pre-1930 historic character. Many areas of Biltmore Village would benefit from reconstruction of building types found in the original plan. Areas should be identified and categorized for their particular type of construction.

2. Identify land use zones where original commercial-type buildings and pre-1930 historic style buildings may be more appropriate today than the re-establishment of the original plan.

Biltmore Village was primarily a residential neighborhood when it was created. Today, it is primarily a commercial district. There are few areas in Biltmore Village where residential structures would be appropriate today. However, the architectural style of the original district should be respected when change of use structures are incorporated.

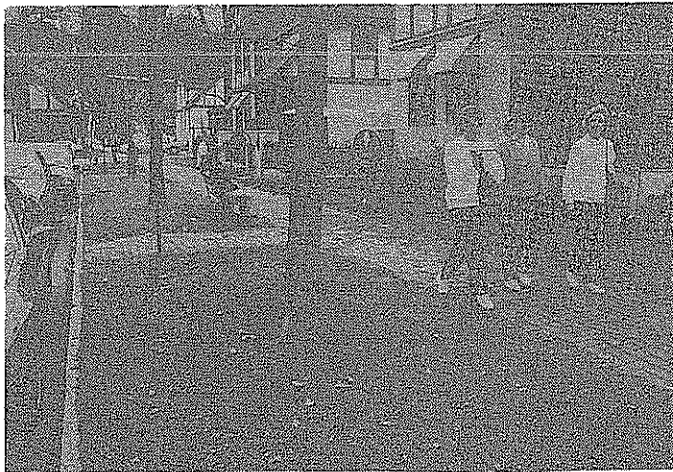
3. Recognize and restore the historic examples of Olmstead's plan including pedestrian amenities, landscape, street patterns, lighting, utilities, street furniture and their relationship to specific architectural styles.

4. Develop a transportation plan to manage the intense volume of vehicular traffic generated by the existing thoroughfare plan and the entrance to the Biltmore Estate®.

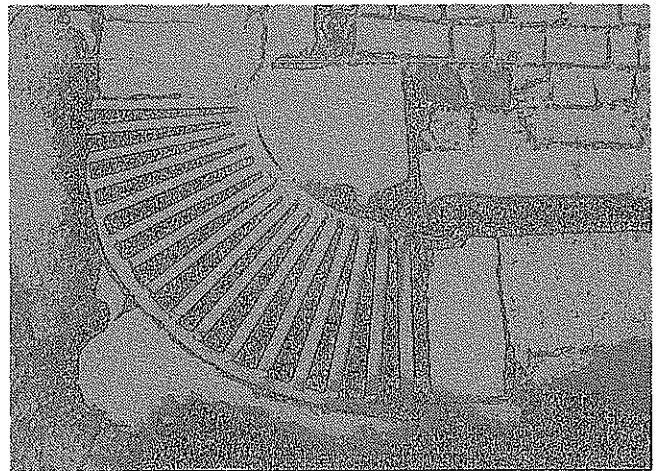
5. Mitigate the negative impact of vehicular traffic on the historic plan and the pedestrian.

HISTORIC PRESERVATION GOALS:

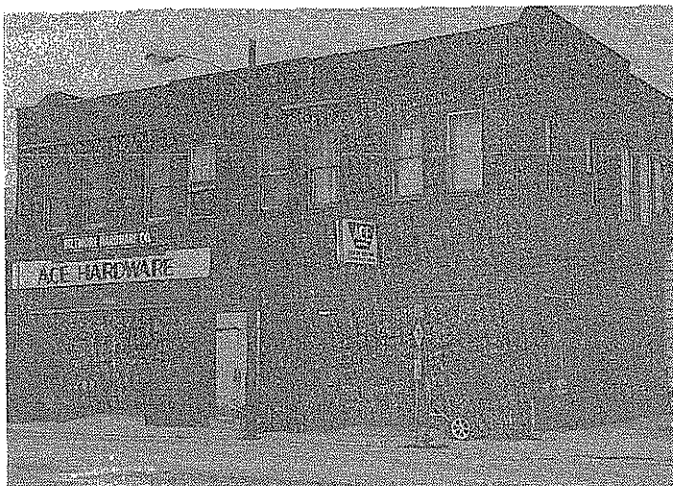
1. Protect and maintain existing historic resources



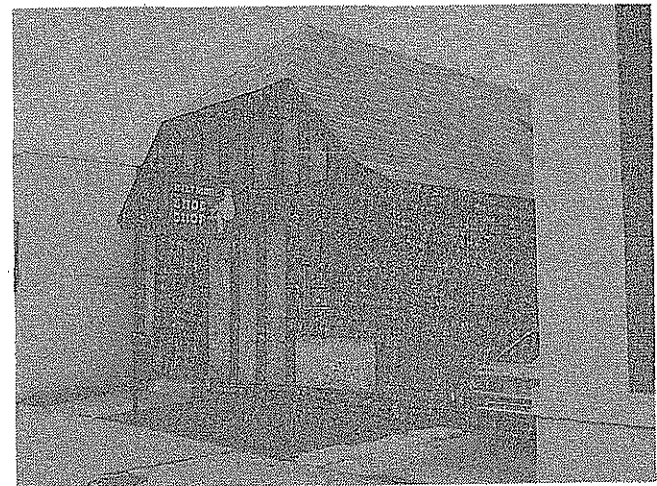
**Granite curbs, trees and
brick sidewalks**



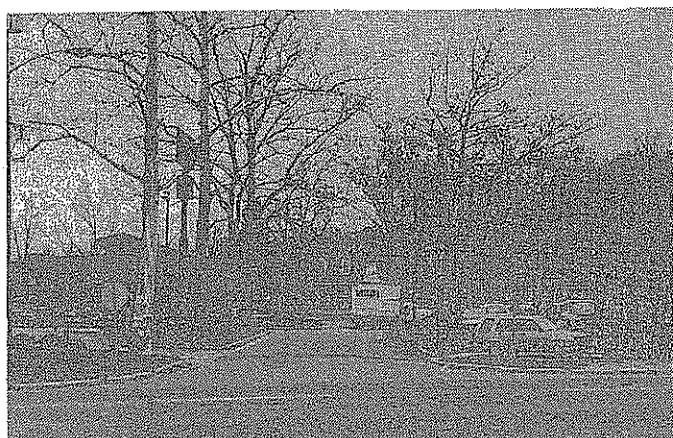
**Granite corner stones and
fan-shaped storm grates**



Preserve pre-1930 structures



Preserve original buildings



Re-plant arcades of trees



Re-use historic bridge designs

HISTORIC PRESERVATION GOALS (CONT.)

2. Reclaim lost elements of the original plan.

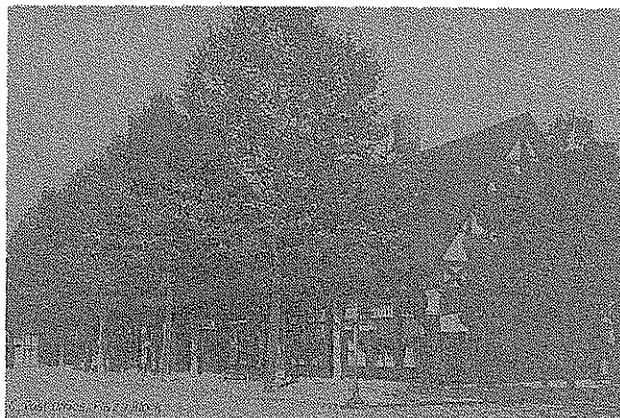


Original Estate entrance



Entrance today

Entrances



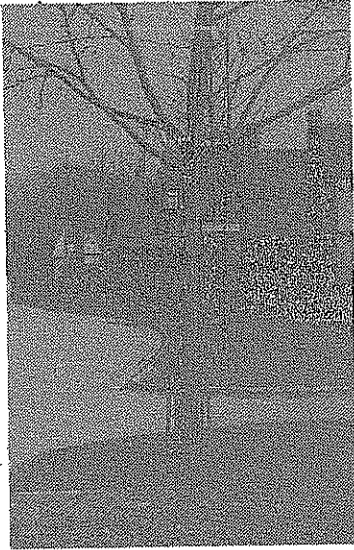
Historic commercial structures



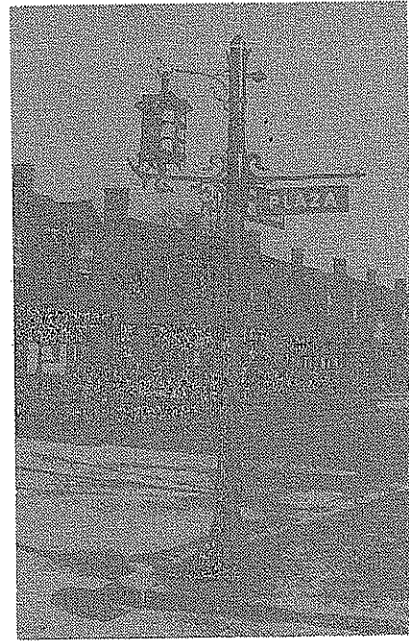
Brick sidewalks

HISTORIC PRESERVATION GOALS (CONT.)

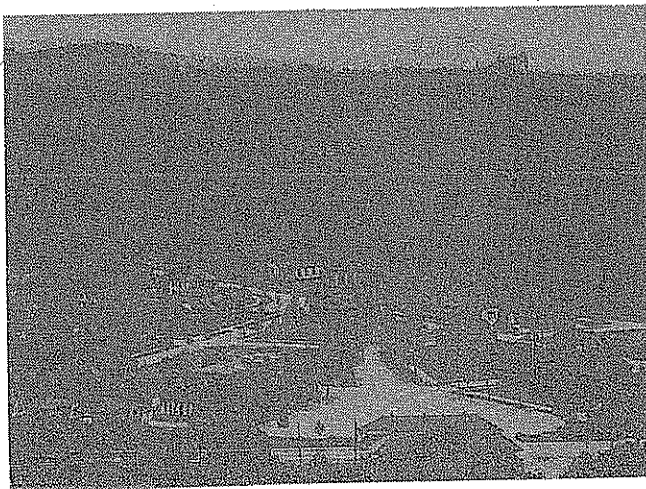
2. Reclaim lost elements of Hunt/Olmsted plan.



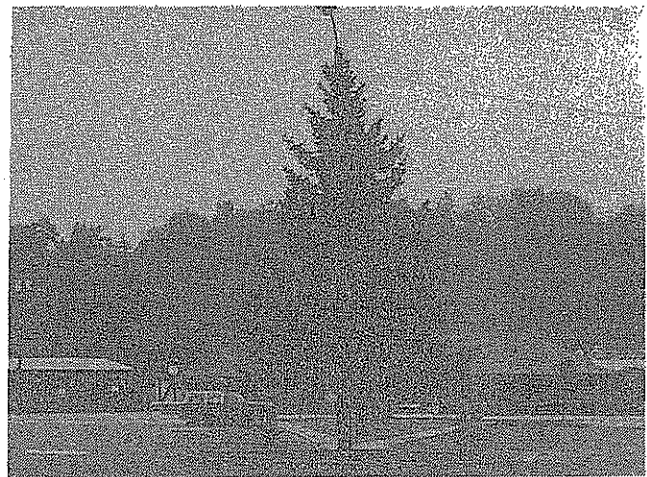
Landscaping



Historic street lights



Original Biltmore Plaza vista



Biltmore Plaza vista 1990

Vistas

SPECIFIC GOALS

COMMITTEE DISCUSSION & PROPOSED SOLUTIONS RE: BILTMORE VILLAGE EXISTING PROBLEMS MAY 1, 1990

Responsible Agencies:

DOT - Dept. of Transportation

Ct - City

Cm - Community

P- Private

(1-20) Priority Years

PROBLEMS

SOLUTIONS

BILTMORE ESTATE ENTRANCE & EXIT PROBLEMS

1. Vehicular Congestion at Entry

1. Internal Welcome Center (1-5) P

2. Left Turn Difficult at Entry

2.a. Limit times to turn left with turn light. DOT
(1-5)

2.b. Encourage DOT improvement of entrance (1-5) Cm

3. Physical design of new bridge to replace existing McDowell Street bridge

3. Encourage Historic design consideration of new bridge and create entry into Biltmore district (1) Cm

GENERAL TRAFFIC PROBLEMS

1. Lack of Parking

1.a. Construct public garage (10-20) Ct

1.b. Employee Lots recommended locations Cm

- Slossman lot N. side Sweeten Creek Rd

- S.R.R. right of way behind Hot Shot etc.

- First Union lot next to All Souls Crescent (1-3)

1.c. Two hour limit for street parking, after employee lot is created (1) Ct

1.d. Ride Share Program (1-5) P

1.e. Shuttle (1) Ct/Cm

1.f. Electronic transactions for Banks (10-20) P

1.g. Shared parking on the interior of lots. (1-3) P/Cm

- | | | |
|---|--|-----|
| 2. Thru Traffic Congestion from non-Biltmore Estate Guest | 2.a. Synchronize Lights (1-3) | DOT |
| | 2.b. Encourage Bilt. Village Bypass, Over/Under Pass (5-10) | DOT |
| | 2.c. Diverted Traffic / Ring Road (1-5) | DOT |
| | 2.d. Encourage Access from 25A to I-40 include Biltmore Village By-Pass (1-5) | DOT |
| | 2.e. Left turn light at West bound Lodge St turning S. onto Hendersonville Highway (1-3) | DOT |
| | 2.f. Enforce No Truck traffic law on All Souls Crescent between 25 & 25A (1-2) | Ct |

SIGNAGE PROBLEMS

- | | | |
|--|--|-----|
| 1. Internally lit tall plastic signs from national franchises dominate & conflict with the historic character of Biltmore Village. | 1. Amortization of plastic internally lit signs or any other existing signs that do not comply with existing historic guidelines. (5-7) | Ct |
| 2. Off Premise signs compete with on premise signs & architecture. | 2.a. Amortize Billboards & other off premise signs (5-7) | Ct |
| | 2.b. Lobby for reduced DOT logo advertising prices extended to businesses in historic district who voluntarily de-escalate signage prior to amortization requirement (1-3) | CM |
| | 2.c. "Persuasion" technique ref: redesign. (1-3) | P |
| 3. Color/Style of some signs are incongruous with historic context | 3.a. Subdue colors of signage (5-7) | P |
| | 3.b. Encourage similar styles of signs (1-3) | Ct |
| 4. Need sign to direct visitors to Bilt. Village & to announce entry. | 4.a. Consider different sign locations (1-3) | P |
| | 4.b. Provide DOT logo signage re: direction signs to Biltmore Village a National Registered District (1-3) | DOT |

STREET/SIDEWALK IMPROVEMENTS

- | | | |
|---|---|----|
| 1. Sidewalk right-of-ways encroachment on | 1. Clear right-of-ways (1-2) | Ct |
| 2. Sidewalks improvement | 2.a. Devise joint venture improvements between the owners & City | Ct |
| | • Establish crushed brick paths where sidewalks are missing (1-2) | |
| | • Repair existing brick sidewalks, maintain old brick (1) | |
| | • Replace concrete sidewalks & crushed brick paths with brick sidewalks (1-5) | |
| | 2.b. Limit curb cuts (1-2) | Ct |

- | | | |
|---|---|----|
| 3. Streets need paving | 3. Scrape & repave road. Consider brick at crosswalks & crushed brick as aggregate (1-5) | Ct |
| 4. Provisions for future utilities should be planned to avoid conflict with future sidewalk & street improvements | 4. Improve subsurface utilities prior to paving. (1-3) | Ct |
| 5. Underground drops to street lights | 5.a. Place conduit for future historic street lights. Coordinate with item 2 B (1) | Ct |
| | 5.b. Request future utility work to be in ground and accessible from permanent locations to avoid disrupting new surface improvements (1-5) | Ct |
| | 5.c. Consolidate above ground utilities (1-5) | Ct |
| 6. Pedestrian/Vehicle conflict | 6.a. Add pedestrian cross walks (1-2) | Ct |
| | 6.b. Add "walk / don't walk" lights (1-5) | Ct |
| | 6.c. Caution signs for pedestrian crossing. (1-2) | Ct |

PUBLIC LANDSCAPING

- | | | |
|--|---|-----|
| 1. Sidewalks on the north side of Sweeten Creek need buffer between street | 1. Reduce width of street to allow planting strip (1-5) | DOT |
|--|---|-----|

EDUCATION

- | | | |
|--|--|----|
| 1. Tenants & Property owners do not understand direction & details of guidelines | 1. Public education ie: Guidelines (1) | CT |
|--|--|----|